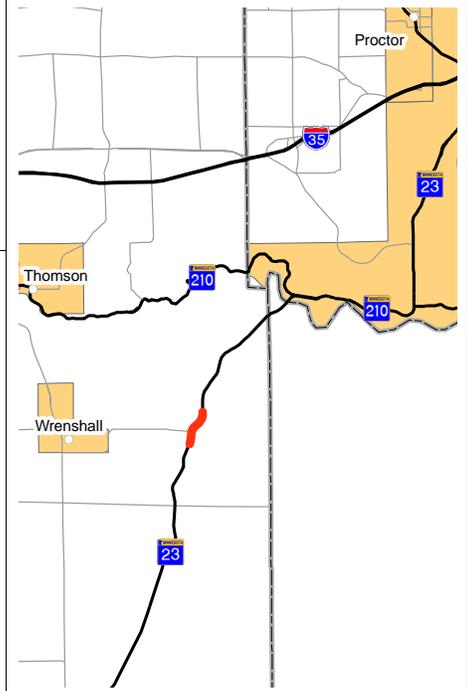


PROJECT SUMMARY

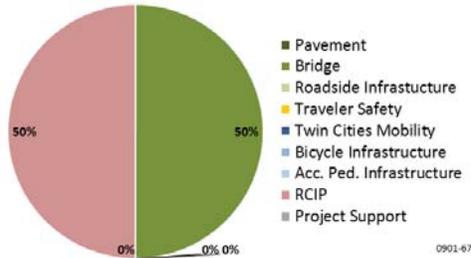
Hwy 23
 Hwy 23 near intersection with County Road 18
 Bridge 5470
 State Project No. 0901-67



Primary Purpose:

Performance-based Need: Bridge condition

Investment Category:



Project Description:

This project replaces the bridge that carries Hwy 23 over the Burlington Northern Santa Fe railroad. The bridge is being constructed on a new alignment so the existing bridge can continue to be used during construction. The project is located approximately 16 miles NE of the south Carlton County line.

Recent Changes and Updates

This two-year project is under construction and on schedule. MnDOT continues to coordinate the development of this project with the BNSF railroad.

Project History:

Bridge 5470 was built in 1936 and consists of a steel beam span with a cast in place concrete deck. This bridge is classified as structurally deficient. In 1973 the bridge received repairs to the deck, abutments, pier caps, and new concrete.

A conceptual sketch was developed and shared with the railroad in August 2013. The project impacts a local township road and MnDOT has met and coordinated with the local government.

Total Project Cost Estimate (millions)

Date in which the project entered into the STIP: 2011

	<u>Baseline Est.</u>	<u>Current Est.</u>
Construction Letting:	\$ 5.0	\$ 3.2
Other Construction Elements:	\$ 0.2	\$ 0.2
Engineering:	\$ 1.0	\$ 0.7
Right of Way:	\$ 0.1	\$ 0.1
Total:	\$ 6.3	\$ 4.2

Construction cost estimates are adjusted to the mid-year of construction, using inflation rates provided by OTSM.

Key Cost Estimate Assumptions:

The project was let in December of 2014. The current estimate is based on the bid cost. The project cost was reduced from the base cost as result of refining the roadway alignment and bridge design.

Project Risks:

The project has been let. Risks remaining include potential construction changes.

Schedule:

Environmental Approval Date: 09/17/2014
 Municipal Consent Approval Date: Not Needed
 Geometric Layout Approval Date: 06/14/2013
 Construction Limits Established Date: 12/26/2013
 Original Letting Date: 06/27/2003
 Current Letting Date: 11/21/2014
 Construction Season: 2015
 Estimated Substantial Completion: Fall 2016



Minnesota Department of Transportation
 District 1
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District Engineer: Duane Hill
Project Manager: Michael Kalnbach

Revised Date: 12/15/2015