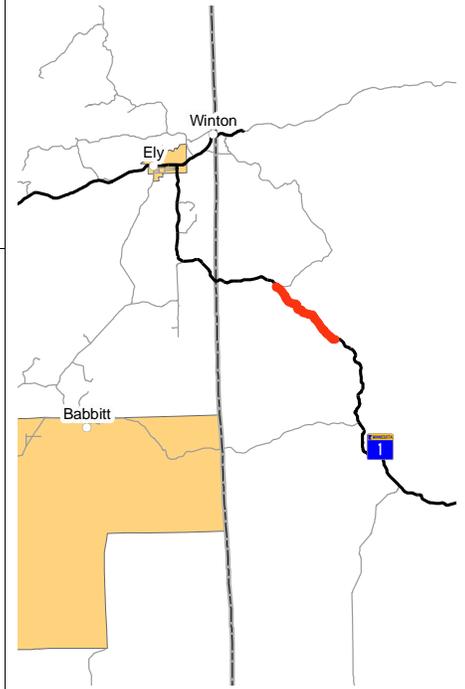


PROJECT SUMMARY

Hwy 1

Located about a half mile north of the Kawishiwi River on Hwy 1 for 5 miles; and, another segment for 14 miles on Hwy 1 from one mile south of CR 2 to Isabella

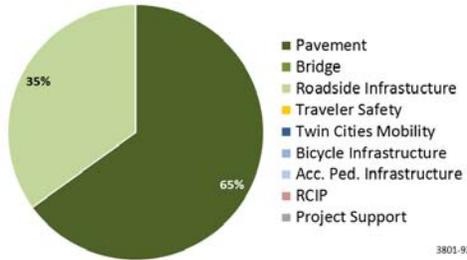
State Project No. 3801-92



Primary Purpose:

Performance-based Need: Pavement condition

Investment Category:



3801-92

Project Description:

The northerly project segment is 5 miles long and located south of the Kawishiwi River. The southerly project segment is 14 miles long and located between Lake County Road 2 and Isabella. The work for both projects includes bituminous resurfacing, drainage improvements and the removal of rock outcroppings in some areas.

Recent Changes and Updates

The project is programmed for construction in 2016. A large portion of the project is located in the Superior National Forest. In this area, MnDOT does not own any highway right-of-way; so, easements will need to be obtained from the Superior National Forest and permanent right-of-way from a number of private land owners.

Project History:

The north project segment was initially planned to be reconstructed, however, funding for reconstruction is no longer available. This segment will now be resurfaced along with drainage improvements and the removal of rock outcroppings in some areas.

Total Project Cost Estimate (millions)

Date in which the project entered into the STIP: 2012

	<u>Baseline Est.</u>	<u>Current Est.</u>
Construction Letting:	\$ 6.6	\$ 5.7
Other Construction Elements:	\$ 0.4	\$ 0.5
Engineering:	\$ 1.4	\$ 1.2
Right of Way:	\$ 0.0	\$ 0.0
Total:	\$ 8.4	\$ 7.4

Construction cost estimates are adjusted to the mid-year of construction, using inflation rates provided by OTSM.

Key Cost Estimate Assumptions:

The current estimate was prepared in February of 2014. It includes the cost for pavement resurfacing and drainage improvements. The cost has been reduced as project scoping has progressed, which showed the need for fewer drainage improvements.

Project Risks:

A substantial amount of culvert replacement work is required. There may be challenges in obtaining highway easements from the Superior National Forest due to federal environmental/NEPA processes.

There is potential for sulfides to exist in the rock outcroppings. If encountered, it could require avoidance or mitigation due to acid drainage runoff.

Schedule:

Environmental Approval Date: Pending Approval
 Municipal Consent Approval Date: Not Needed
 Geometric Layout Approval Date: Not Needed
 Construction Limits Established Date: Pending Approval
 Original Letting Date: 02/27/2012
 Current Letting Date: 2/24/2017
 Construction Season: 2017
 Estimated Substantial Completion: Fall 2017



Minnesota Department of Transportation
 District 1
 1123 Mesaba Ave
 (218) 725-2700

District Engineer: Duane Hill
Project Manager: Brian Larson

Revised Date: 12/15/2015