

ORR ROADSIDE PARKING AREA

SHPO INV. # SL-ORC-005

Location: The overlook wall is on the western side of T.H. 53, 1.6 miles north of the junction of T.H. 53 and CSAH 23 in St. Louis County, City of Orr, MN.

Introduction: The Civilian Conservation Corps as a part of a state and nationwide program constructed the historic roadside rest in 1938 in cooperation with the highway department. The entire site is about 1,900 feet long, adjacent to the eastern shore of Pelican Lake, and extends from the MnDNR forestry station to the picnic area on the south. The wall was not completed in accordance with the design plans. No steps lead from the wall to the dock as designed; just a gravel ramp exists in this location. The overlook wall is in poor condition—a stone section and wood railings are missing and the southern section is leaning toward the lake and pulling away from the corner return wall.

Architect's Survey Date: September 15, 1999

Plans/Sketches (Attached):

1. 1937 Tentative Sketch, MHS-site
2. 10/37 Plans for Roadside Camp Development
3. MJBA Sketches—Existing Conditions as of 9/99
 - a. Site Plan/Wall Plan
 - b. East and West Wall Elevations
4. Barr Engineering Field Observations and Recommendations
5. MJBA Recommendations

**MNDOT HISTORIC ROADSIDE DEVELOPMENT
STRUCTURES INVENTORY**

SL-ORC-005
CS 6922
Orr Roadside Parking Area

Historic Name Other Name	Orr Roadside Parking Area	CS # SHPO Inv #	6922 SL-ORC-005
Location	W side of TH 53 1.6 mi N of the jct of TH 53 and CSAH 23 (jct is south of Orr)	Hwy District Reference	TH 53 1B 110
City/Township County Twp Rng Sec USGS Quad UTM	Orr, City of St. Louis 64N 20W Sec 1 Orr Z15 E512610 N5322550	Acres Rest Area Class	5 10 4
Designer	Nichols, A R, Consult Land Arch	SP #	6922-07 53-11-39-3
Builder	Civilian Conservation Corps (CCC)	SHPO Review #	
Historic Use Present Use	Roadside Parking Area Roadside Parking Area	MHS Photo #	013558.15-25
Yr of Landscape Design	1938	MnDOT Historic Photo Album	Nic 1.17 Ols 3.135
Overall Site Integrity	Intact/Slightly Altered		
Review Required	Yes		
National Register Status	Eligible, see Statement of Significance Now listed: see http://www.dot.state.mn.us/tecsup/site/historic/files/wayrep-suppl.pdf for additional information.		
Historic Context	Roadside Development on Minnesota Trunk Highways, 1920-1960		

List of Standing Structures

Feat#	Feature Type	Year Built	Fieldwork Date
01	Overlook Wall	1938	08-10-97
02	Picnic Shelter(s)	Ca. 1980	
03	Footbridge	1989	
04	Dock	1989	
NOTE: Landscape features are not listed in this table			Prep by Gemini Research Dec. 98 G1. 53
			Prep for Site Development Unit Cultural Resources Unit Environmental Studies Unit

Final Report	Historic Roadside Development Structures on Minnesota Trunk Highways (1998)
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Stabilization/Preservation/Restoration

1. Spatial Organization and Land Patterns

a. Functional Relationships:

- Assessment: The Orr Roadside Parking Area is a relatively long, narrow parcel of land that is located on the eastern shore of Pelican Lake. The site includes land on both sides of T.H. 53. However, only the parkland on the western side of the highway is currently in use.

A "Tentative Sketch" of the site (drawn July 7, 1937) provides a plan of the site as it was originally conceived. The Sketch divides the site into several use areas, most designed to provide access to, or views of, Pelican Lake. Most of the areas were originally linked by walking trails. On the western side of T.H. 53 (near the site's north-south midpoint) is drawn the park's most prominent feature, the stone overlook wall (with adjacent parking area), as well as an existing dock. South of the overlook wall is drawn a linear, shoreline picnic area. South of the picnic area is a swimming beach with a parking area and a "proposed bath house." The parkland east of T.H. 53 is drawn to include, at its midpoint, a picnic area with a council ring, small stone overlook, and a shallow parking area. South of the council ring is drawn a north-south hiking trail. North of the council ring on the sketch is a three-building Department of Conservation forestry station. The forestry station is shown encircled by hiking trails. There is a third picnic area tucked between the forestry station and the highway.

Most of the site was built as planned on the Tentative Sketch, with a few exceptions. On the western side of T.H. 53, the overlook wall was not built exactly as planned and two small changing houses were built instead of the bathhouse. On the eastern side of T.H. 53, the eastern picnic area and the Department of Conservation forestry station were built, although the forestry station's layout was different than that shown and did not include a third picnic area. It is presumed that the trails on the eastern side of T.H. 53 were created. The council ring and smaller stone overlook were apparently not built.

Today, the size, spatial organization, and use of the parkland on the western side of T.H. 53 have been maintained with only a few changes. Most notably, the picnic area has been shifted southward (closer to the southern parking area), leaving the land between the overlook wall and the swimming beach as a linear, shoreline walking area. A log picnic shelter (with two log tables), fixed metal benches, and playground equipment have been added near the southern parking area. The City maintains this part of the site. At the overlook wall, an interpretive marker about the CCC has been added. The wall had no previous plaques except a cornerstone.

On the eastern side of T.H. 53, the picnic area (also used as a 15-unit public camping area) was closed in the early 1970's and the land is currently overgrown and unused. The forestry station appears to be largely intact in plan, although this facility is not currently tied functionally to the western side of T.H. 53 and is probably not visited by rest area users.

- Brief Description of the Department of Conservation Forestry Station

The Department of Conservation Forestry Station (now MnDNR Area Headquarters) was built in 1938-1939 by the Cusson CCC Camp, the same CCC camp that built the wayside rest. The station originally had about five buildings including an office, bunkhouse, warehouse, garage, and an oil house. (For a plan of the station, see "Orr Ranger Station, Administrative Site-Ground Plan, drawn 8-15-38, updated 1940, 1945, 1946, 1952, 1954, 1956, File FO-632, MnDNR). Many of these buildings are probably extant. At the western edge of the property, adjacent to T.H. 53, is the station's original semicircular asphalt-paved pull-off drive. Adjacent to the drive is a 65'-long, waist-high stone retaining wall that was built circa 1938-1939. Very similar to the Orr Overlook Wall, the MnDNR stone

wall is built of randomly laid, roughly-cut gray granite with deep mortar joints. The wall has a stone stairway near each end, allowing visitors to pass through the wall and climb the hillside to the office. Linking the stairways and the office are two 50'-long, 6'-wide flagstone walkways that lead up the hillside. Both the wall and the flagstone walkways are intact. The forestry station has an excellent view of Pelican Lake. Its buildings are sited on a wooded hillside that rises eastward above T.H. 53. The MnDNR stone wall and the Mn/DOT stone overlook wall are about 600' feet apart and within clear site of each other.

- **Recommendations**

Stabilization: None

Preservation: Reclaim through planting and maintenance the land on the eastern side of T.H. 53 as part of the park. Work period: 3 - 5 years (to protect from development, etc.).

Restoration: Replace the current picnic tables in the southwestern picnic area with portable wooden tables based on the Roadside Development Division's standard portable wooden picnic table design of the 1930s-40s. If a hard-surfaced path to a table is necessary, create a path and/or pad using a material like "Grass-crete".

Reclaim through planting and maintenance the land on the eastern side of T.H. 53 as part of the park. Reestablish hiking trails on the eastern side of T.H. 53 to reestablish links to the MnDNR forestry station. Work with the MnDNR and the City of Orr to jointly protect and manage the area. Work period: 3 - 5 years.

b. **Visual Relationships:**

- **Assessment:** The setting has changed little since the site was built. The site is located north of downtown Orr. It is surrounded by the lake on the west, forests and T.H. 53 on the north, a residence on the south, and the former Duluth, Winnipeg, and Pacific railroad tracks (which predate the site) on the east. Beyond the railroad tracks is a rocky, forested, lightly developed hillside.

It was originally intended that Pelican Lake be viewed from nearly all portions of the site. (The elevation of the site rises from west to east, so that even the parkland on the eastern side of T.H. 53 has excellent lake views.) Today, the site retains its original lake views. The lakeshore that can be seen from the site is largely forested and lightly developed with well-screened lake cottages.

Views to the north of the site are generally intact, as are views of one well-screened house and the town's central business district to the south. Views to the east include the railroad tracks, a forested hillside, and one recent apartment building. A rock outcropping on the eastern T.H. 53 shoulder (within the site, southeast of the overlook wall) is visually important to the site. Future residential and commercial development southeast and east of the site is likely.

Views toward the rest area from the surrounding lands were originally partly screened by trees. These views of the site are basically intact.

- **Recommendations:**

Stabilization: None.

Preservation: Selectively cut a few trees and some brush along the northern portion of the overlook wall to reopen the lake view. Preserve the view between the overlook wall and the MnDNR forestry station's stone wall so that the two support one another visually. Work period: as soon as possible.

Restoration: Work with the MnDNR and the City of Orr to help protect viewsheds in the area. Work period: as soon as possible.

2. Topography

- **Assessment/Recommendations:** The parking area and the paths along the lake's edge are basically flat. The slope from the overlook wall to the water is steep. Surround-

ing the site are rolling hills that are heavily covered with trees and vegetation. Drainage on the site is good. No significant erosion is visible. No recommendations.

3. Vegetation

- Assessment: The site was designed to be a wooded, rocky lakeshore park. No original planting plans or good historic photos showing early plantings have been identified. (An exception is a June 30, 1941, photo that shows existing trees between the overlook wall and the shore.) The 1937 Tentative Sketch shows what are probably extensive existing trees, as well as trees and shrubs that were probably added or reestablished along the overlook wall and at other site features to blend them with the landscape.

Today, the park land west of T.H. 53 has dense screens of mature deciduous and evergreen trees at the northern and southern ends. There are scattered mature and young trees along the lakeshore, and a screen of trees and brush between the shoreline walking trail and T.H. 53. The overlook wall has mature trees at either end and between the wall and the lakeshore. There is mowed grass around the overlook wall, along the walking trail, and in the picnic and swimming area. There is an important line of natural boulders along the trail that reflects the rocky lakeshore.

The park land east of T.H. 53 and south of the forestry station is covered with overgrown trees and brush. There has been recent cutting, clearing, and scarring from utility cable burying. The forestry station is moderately wooded and has mowed grass.

- Recommendations:
Stabilization: Selectively cut a few trees and some brush along the northern portion of the overlook wall to reopen the lake view. Clear trees and brush from the northern end of the wall to a distance of about 3'. Trim all vegetation from wall's walkway. **Work Period:** ASAP.
Preservation: Selectively cut a few trees and some brush along the northern portion of the overlook wall to reopen the lake view. Clear trees and brush from the northern end of the wall to a distance of about 3'. Trim all vegetation from wall's walkway. Plant additional trees, including some evergreens, in the southern picnic area which is now more open than intended by the original design. Repair cutting and clearing damage on the eastern side of T.H. 53 to reclaim this land as part of the park. **Work period:** as soon as possible.
Use naturalistic plantings of local trees, shrubs and/or plants to erase construction scars, restore the landscape, and support the reestablishment of a hiking trail east of T.H. 53 with views of the lake. Establish and follow a regular schedule of fertilizing, mowing, pruning, and trimming. **Work period:** 3 - 5 years.

Restoration: Selectively cut a few trees and some brush along the northern portion of the overlook wall to reopen the lake view. Clear trees and brush from the northern end of the wall to a distance of about 3'. Trim all vegetation from wall's walkway. Plant additional trees, including some evergreens, in the southern picnic area which is now more open than intended by the original design. Screen the southern parking area from T.H. 53, using naturalistic plantings of trees and shrubs similar to those existing on the site. Repair and replant the land on the eastern side of T.H. 53 to reclaim this area as part of the park. **Work period:** as soon as possible.

Use naturalistic plantings of local trees, shrubs and/or plants to erase construction scars, restore the landscape, and support the reestablishment of a hiking trail east of T.H. 53 with views of the lake. Establish and follow a regular maintenance schedule of fertilizing, mowing, pruning, and trimming. Work with the MnDNR and the City of Orr to protect and maintain vegetation. **Work period:** 3 - 5 years.

4. Circulation

a. Roads

- **Assessment:** T.H. 53 bisects the site. The highway was widened slightly around the early 1970's. Traffic on the highway is often heavy and travels at about 50 mph, considerably faster than when the site first opened. The traffic noise and motion are intrusive to visitors at the overlook wall, but the shoreline trail and southwestern picnic area are at a lower elevation and screened by trees. The speed of the traffic makes turning into the overlook wall's parking area and the MnDNR pull-off drives somewhat difficult. A modern metal highway guardrail extends along T.H. 53 through much of the site.

The site's only internal roads are a short semicircular pull-off drive at the forestry station (see Parking below) and a longer MnDNR service drive that enters the forestry station from T.H. 53 at points both north and south of the pull-off drive.

- **Recommendations:**

Stabilization: None

Preservation: None

Restoration: Replace the modern metal guardrail with a design more compatible with park use (see for example, FHWA Report FHWA-SA-91-051). Consider reducing the highway speed limit past the site and instituting a no-passing zone to increase safety and enjoyment by visitors. Add advance highway signs to allow for adequate decision making and time to slow down before entering site or reevaluate the positioning of such signs if they exist. **Work period:** 3 - 5 years.

b. Parking areas

- **Assessment:** The site as drawn on the 1937 Tentative Sketch has four parking areas: one at the overlook wall, one at the southern swimming beach, a shallow parking area to serve the eastern picnic area, and a parking area at the forestry station. Rustic Timber Guardrails surround the southern and eastern parking areas on the Sketch.

Today, the overlook wall parking area is paved with asphalt. Because of subsequent repavings, the curb face of the overlook's flagstone walkway is nearly obscured. The southern parking area has also been paved with asphalt. No Rustic Timber Guardrail is in place, but a few concrete dummy posts and two Jersey barriers stand along its edge. The eastern picnic area and its parking lot are gone. At the forestry station, the semicircular pull-off drive doubles as a parking area. It also has been paved with asphalt.

- **Recommendations:**

Stabilization: None

Preservation: None

Restoration: Restore the original curb depth of the overlook wall parking area. Remove the two Jersey barriers and concrete dummy posts from the southern parking area and restore the Rustic Timber Guardrail around its perimeter. (Use the Roadside Development Division's standard Rustic Timber Guardrail design of the 1930s-40s.) Work with the MnDNR to protect the design integrity of the forestry station's pull-off drive. **Work period:** 3 - 5 years.

c. Pedestrian Walks

- **Assessment:** The overlook wall was designed with a 5'-wide flagstone walkway (with stone curbing) along its inner face. The southern 40' of flagstone has been removed and replaced with concrete. The remaining flagstone has heaved and/or settled in several locations and its edges are overgrown with weeds.

The Orr site as drawn on the 1937 Tentative Sketch also has extensive walking trails that run along the shore of Pelican Lake west of T.H. 53 and along the wooded hillside east of T.H. 53. On the Sketch, the trails also loop around the forestry station. A "Proposed Underpass" at the northern end of the overlook wall links trails on the eastern and western sides of T.H. 53.

Today, there is still a gravel trail on the western side of T.H. 53 that links the overlook wall and the southern end of the site. On the eastern side of T.H.

53, cutting and clearing scars from recent utility cable work occupy much of area of the southern trails. It is not known whether the trails around the forestry station were ever created, or if they exist today.

Pedestrians can walk to the southern end of the site from the residential and commercial portions of Orr. Walking north from the overlook wall on the western side of T.H. 53 should probably be discouraged because of the steep topography and close highway traffic. A trail leading north from the forestry station on the eastern side of T.H. 53 is drawn on the Tentative Sketch but apparently does not exist.

- **Recommendations:**

Stabilization: None

Preservation: Clear all weeds from the flagstone walkway. Work period: as soon as possible.

Restoration: Remove the concrete and flagstone from the walkway, regrade the sand base, and reinstall flagstone. Replace missing pieces with matching stone and replicate original mortar treatment. Provide a sensitively designed curb cut near one end of the walkway for wheelchair access. Work period: 1 - 3 years.

Restore the hiking trail along the eastern side of T.H. 53 to link the southern end of the site with the MnDNR forestry station. Work with the MnDNR and the City of Orr to protect and maintain the site's trails. Work period: 3 - 5 years.

5. Water Features: Not applicable

6. Structures, Furnishings and Objects

a. Bench(es), Other:

- **Assessment:** Non-historical and unobtrusive fixed benches exist in the picnic area.

- **Recommendations:** Prohibit any more benches.

b. Curb, stone

- **Assessment:** Granite curbing along the wall's walk is in fair condition; however some of it is quite obscured by the high asphalt elevation that abuts it.

- **Recommendations:**

Stabilization: None.

Preservation: Not appropriate for this area due to ADA requirements.

Restoration: Remove and reset the edge to match the required elevation of the flagstone walk and meet the gradient of the parking area that is required. This is required to improve drainage, protect public safety and meet ADA requirements. Handicapped access shall be accomplished by raising the elevation of the asphalt improvements at the south end of the walk to eliminate the need for a "curb cut". Work period: as soon as possible.

c. Dock: (Fishing Pier):

- **Assessment:** Non-historical, but in location of the original dock.
- **Recommendation:** Keep all future docks in this same location.

d. Fireplace: (Picnic Area):

- **Assessment:** One original fireplace and the circular concrete bases of several others.
- **Recommendation:** No recommendations. If more cooking in the southwestern picnic area is desired, restore swiveliling metal picnic fireplaces on their concrete bases to match the intact base/fireplace.

e. Footbridge: (Picnic Area):

- Assessment: Non-historical
 - Recommendation: No recommendations as condition is good. Optional restoration recommendation is to replace the modern footbridge with reconstruction of original footbridge.
- f. Overlook Wall **Note: overlook wall rehabilitated in 2002**
- Assessment: The wall was not built according to either the preliminary or final design plans. It's in poor condition. A section of stone directly south of the gravel ramp has collapsed. The log railings in the next section are missing. Both sections are surrounded by orange construction fence. The next 70 feet of wall leans toward the water and has pulled away from the curved return wall leaving a gap of about 3". Refer to Barr Engineering's report and drawing. Various other sections of the wall have either single or multiple stones missing. The mortar (grout) is in poor condition. The top of the wall is covered by moss, thick in some locations, thus weakening the mortar. The incised data stone marker is in good condition. Dirt fill has been placed at the southern end of the wall to presumably prevent the wall from overturning any further.

The original source of stone is from nearby gravel pit.
 - Recommendations:
Stabilization: Structurally pin the leaning sections of wall to prevent further failure. This may be nearly the same amount of work that Preservation will entail. Work period: 1 - 3 years.
Preservation: structurally secure the wall; rebuild missing portion of the stone wall, install new matching wooden logs. Remove sections of failing mortar joints and repoint; install missing stones with those that remain on site; clean off moss and repoint top of wall. All mortar shall match existing in color, compressive strength and texture. Install new stones that match existing in color size and form—these units should be selectively determined if possible to obscure their replacement in the original wall. Reinstall missing log rails and restore as required for strength and safety and provide appropriate stain to match original color. Remove paint or stain coating from other logs and stain to match the original color. Remove extra fill and berm at southern end of wall following the preservation work. Work period: 1 - 3 years.
Restoration: Remove entire southern straight section of wall salvaging all granite veneer. Stabilize the grade and compacted backfill and fill; pour new foundations; improve the drainage on the high side of the wall and provide an outlet for the water to flow; install the salvaged granite veneer onto the new wall insuring that the pattern and mortar joints match the original wall's. Make only necessary repairs to the remaining wall. Provide newly secured log bearing plates and anchors and replace all logs with new ones painting to match. This recommendation will provide the best long-term solution and maintain the historic appearance and size of the original wall and its design. Remove extra fill and berm at southern end of wall following the preservation work. Work period: 3 - 5 years.
- g. Picnic Shelter(s): Non-historical, but in location of bathhouse on plan thereby consistent with spatial design intent; no recommendation.
- h. Picnic Tables(s), other: Non-historical portable wooden tables; no recommendation—good condition. If restoration is desired, replace the current picnic tables with portable wooden tables based on the Roadside Development Division's standard portable wooden picnic table design of the 1930s-40s. Provide a hard-surfaced path to a table to create a path and/or pad using a material like "Grass-crete".
- i. Other feature (Incised Data Stone)
- Assessment: Stone is in good condition
 - Recommendations:
Preservation/Restoration: Repair joints around the stone to securely anchor the marker in place. Do not move it. Work period: 3 - 5 years.

Stabilization: None

j. Sidewalk: This item is addressed in the pedestrian walks above, Item 4.c.

7. Accessibility Considerations

- **Assessment:** Access to the flagstone walk is difficult and does not comply with ADA. An 8" step at the walk's edge and the gravel ramp prevents the safe use of this by the physically disabled. The grade of the gravel ramp does not comply with either MN Building Codes or ADA. The trail is natural turf and cannot be easily used by a wheelchair-bound individual.
- **Recommendations:** Provide a sensitively designed curb cut onto the flagstone walk only. An alternative is to raise the new asphalt at the southern corner of the wall and walk to provide wheelchair access to the walk surface. Do not provide access to the water via the gravel ramp due to gradient and safety. **Work period:** 1 - 3 years.

8. **Health and Safety Considerations:** Not applicable

9. **Environmental Considerations:** Not applicable

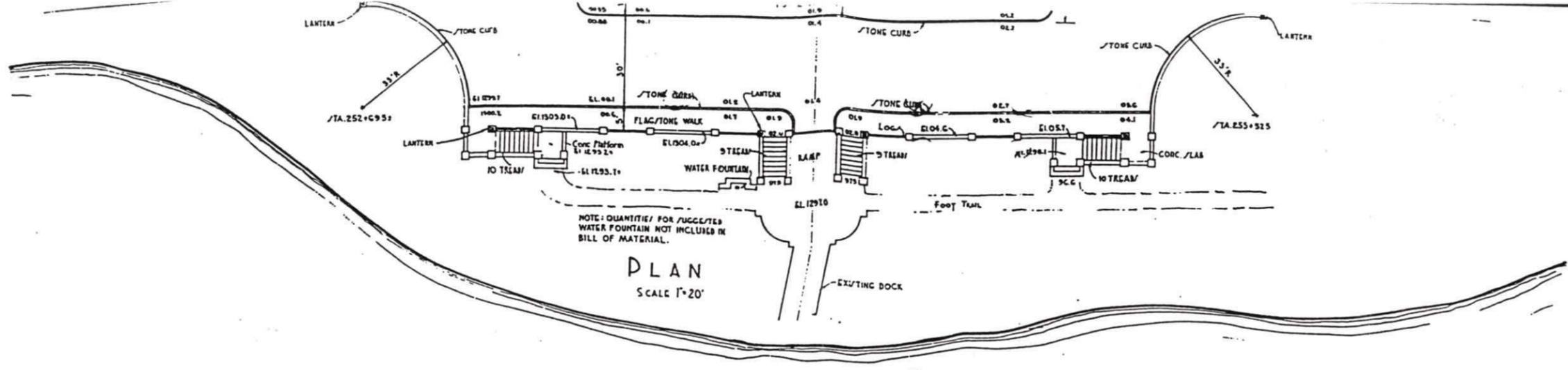
10. Other Considerations/Recommendations:

- **Assessment:** If an interpretive panel that describes the history of the site and its designers and builders is desired, revise the new CCC marker at the overlook wall. The marker could include a diagram of the entire site. Make the revised panel simple and unobtrusive in design. Place it at the southern curved wall. Like the current marker, allow it to be readable from the parking area, thus eliminating the need for an accessibility path or pad.
- **Recommendations:** Signage is recommended to be done as soon as possible to raise the public's awareness of this site's historic importance and educational value. Continue to maintain a garbage receptacle at the southern parking area to help visitors to keep it clean and to discourage dumping and vandalism. Consider adding a receptacle to the overlook area. (Make it portable, simple and unobtrusive in design, and place it near one end of the wall.)

Continue to maintain a privy at the southwestern picnic area. (There is currently a porta-potty there.) The Orr Roadside Parking Area originally had four privies.

11. **Conclusion:** The site is a relatively well maintained rest stop, which enhances the beauty of the scenic overlook. Its careful and thoughtful design and its representation of when "times were really tough" necessitate the careful preservation or restoration of the site. The land on the east side of T.H. 53 should be returned to use. The overlook wall is very visible from T.H. 53, which encourages travelers to stop and "check the area out." It is imperative that the wall be stabilized to prevent further rotation and shifting as well protecting the public's safety. Through careful and sensitive planning the wall can be stabilized and restored to original appearance, while providing safe use and handicapped access to the walkway.

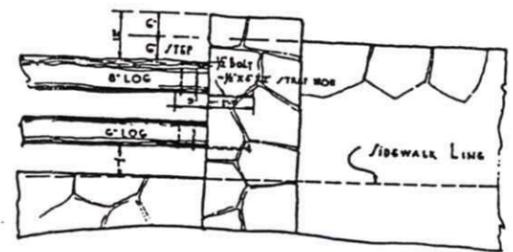
	Stabilization	Preservation	Restoration
Spatial Organization and			
Off-site impacts			
Functional relationships			
Visual relationships			
Cultural landscape limits (land			
Topography			
Character-defining feature			
Non-contributing corrective			
Vegetation	\$2,829	\$20,869	\$38,570
Circulation			
Access road and internal road-			
Parking areas			\$63,782
Pedestrian walks		\$23,275	\$24,959
Paths and trails			
Water Features			
Structures, Furnishings and			
Bath house			
Bench(es), other			
Bench(es), stone			
Bridge/culvert			
Cave			
Council ring			
Curb, stone		\$8,906	\$8,906
Curb, concrete			
Dam			
Dock			
Drinking fountain(s)			
Entrance Wall			
Fireplace(s), other			
Fireplace(s), stone			
Flagpole(s), other			
Flagpole(s), stone			
Flagstone pad			
Footbridge			
Foundation of building			
Gravestone			
Guardrail, stone			
Info board			
Info booth			
Marker			
Other feature (Guardrail re-			\$11,440
Overlook wall	\$119,710	\$125,180	\$155,931
Picnic shelter(s)			
Picnic table(s), other			\$2,640
Picnic table(s), stone			
Privies			
Refuse container(s), stone			
Restroom building			
Retaining wall			
Rock garden			
Sea wall			
Sidewalk			
Signpost, other			
Signpost, stone			
Spring water outlet			
Statue			
Storage building			
Trail steps			
Wall			
Well/pump			
Accessibility Considerations		\$1,408	\$1,408
Health and Safety Considera-			
Environmental Considera-			
Other Considerations (sign-	\$11,616	\$11,616	\$27,455
ESTIMATED COSTS	\$134,155.00	\$191,254.00	\$335,091.00



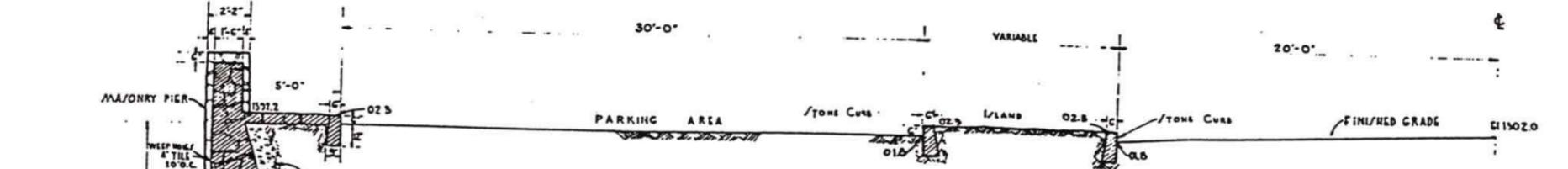
BILL OF MATERIALS	
ITEM	QUANTITY
/TONE	365
CEMENT	1026
/AND	108
GRAVEL	16
LOG/ 8" x 12"-0"	6
LOG/ 8" x 4"-0"	1
LOG/ 6" x 12"-0"	2
LOG/ 6" x 4"-0"	6
REINFORCING BAR/ 1/2"	1170
/TRAP IRON 1/2" x 4" x 2"-0"	24
BOLT/ 1/2" x 1"-0"	48
VITRIFIED TILE (4")	87
/AND CURBION	25
LANTERN COMPLETE WITH TANKS	6
3/4" IRON CONDUIT - LENGTH TO BE DETERMINED IN FIELD	

NOTE: QUANTITIES FOR SUGGESTED WATER FOUNTAIN NOT INCLUDED IN BILL OF MATERIAL.

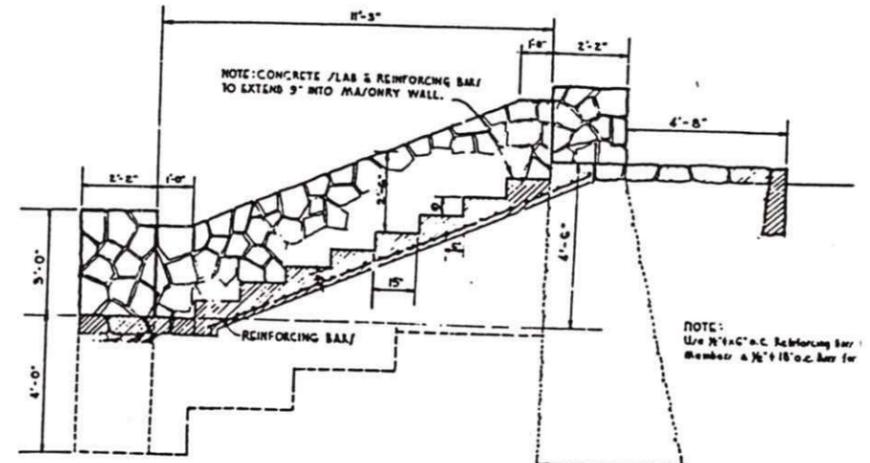
PLAN SCALE 1"=20'



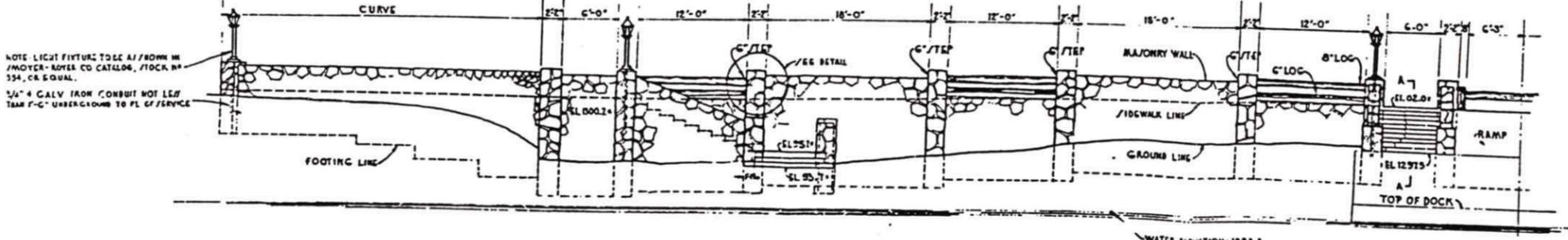
DETAIL OF STEP IN WALL SCALE 1/2"=1'-0"



CROSS SECTION - PARKING AREA (STA 253+70) SCALE 1/8"=1'-0"



SECTION THRU A-A SCALE 1/8"=1'-0"



HALF ELEVATION FROM LAKE SCALE 1/8"=1'-0"

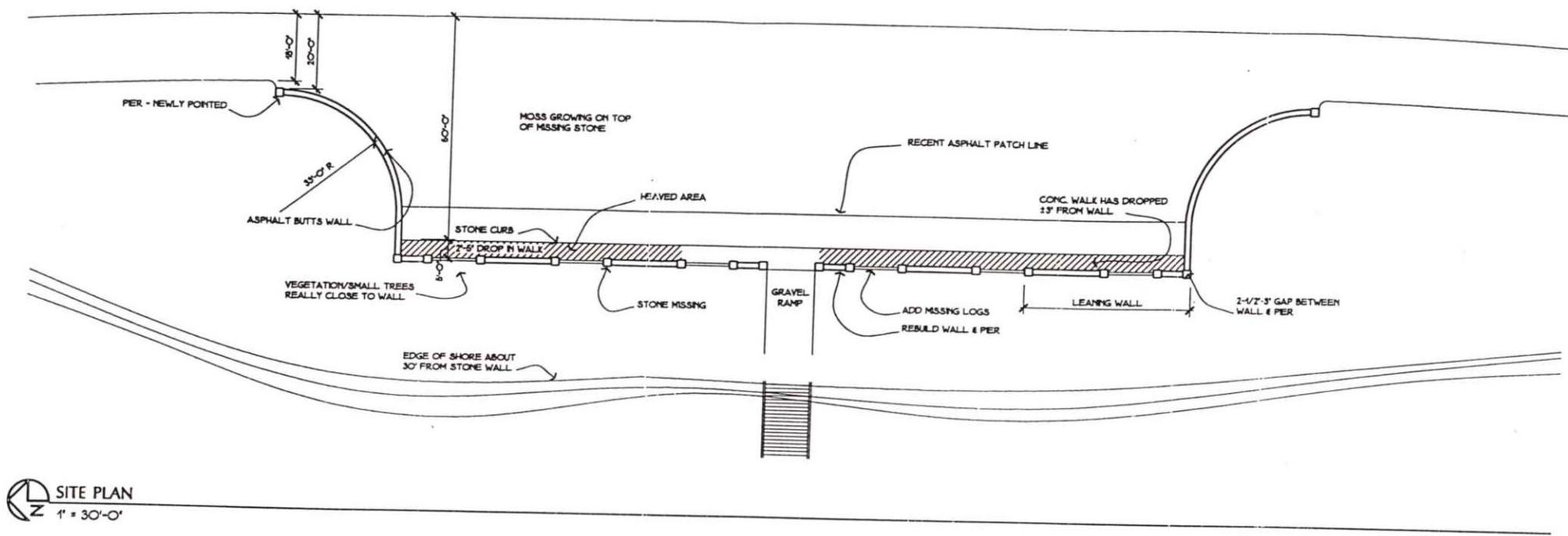
ROAD/IDE DEVELOPMENT
 RECOMMENDED FOR APPROVAL
 [Signature]
 ENGINEER OF SURVEY & DEVELOPMENT CONSULTING ENGINEER
 APPROVED
 [Signature]
 CONSTRUCTION SUPERVISOR

S.P. 6922-07 (TH 03/24)

11-1937

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12/14/99



SITE PLAN
1" = 30'-0"

Orr Roadside Parking Area

SL-ORC-005

FIELD SURVEY OBSERVATIONS

SITE PLAN


 A ARCHITECTURE
 & INTERIOR DESIGN
 & INTERIOR
 PRESERVATION
 MICHAEL J. BURNETT ARCHITECTS P.C.
 1000 WEST 10TH AVENUE, SUITE 1000 DENVER, CO 80202
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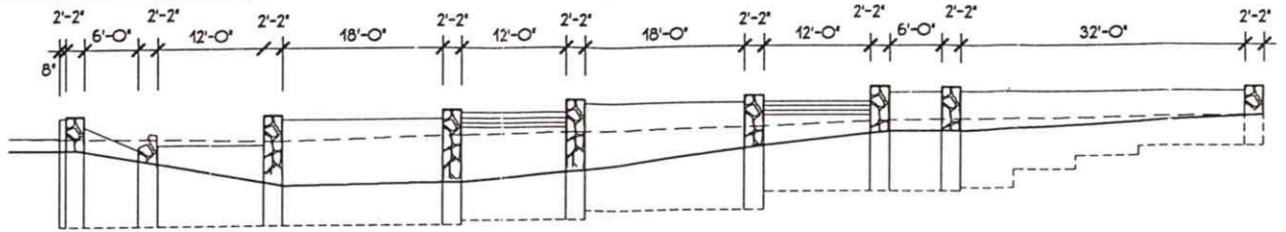
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Date
DEC. 99

Project No.
MJBA 9919

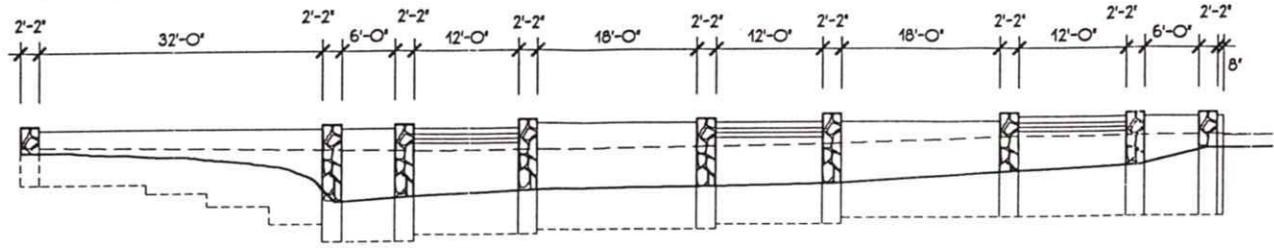
Sheet No.
FS.1

12/14/99



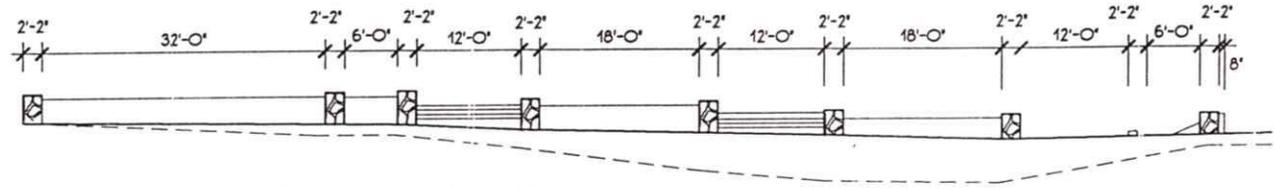
WEST ELEVATION - VIEW FROM LAKE (SOUTH SIDE)

1/16" = 1'-0"



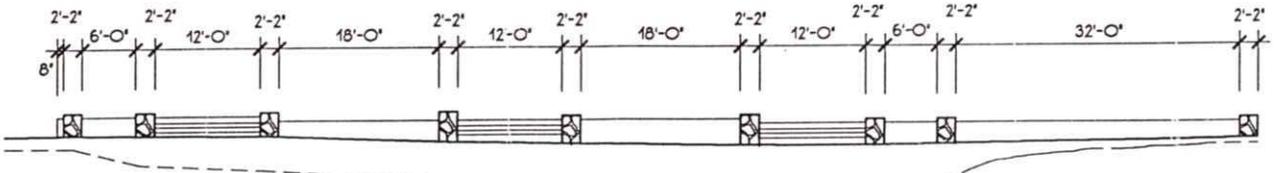
WEST ELEVATION - VIEW FROM LAKE (NORTH SIDE)

1/16" = 1'-0"



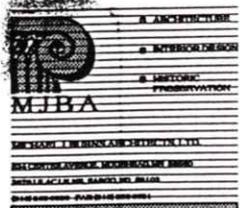
EAST ELEVATION - VIEW FROM ROAD (SOUTH SIDE)

1/16" = 1'-0"



EAST ELEVATION - VIEW FROM ROAD (NORTH SIDE)

1/16" = 1'-0"



Orr Roadside Parking Area

SL-ORC-005

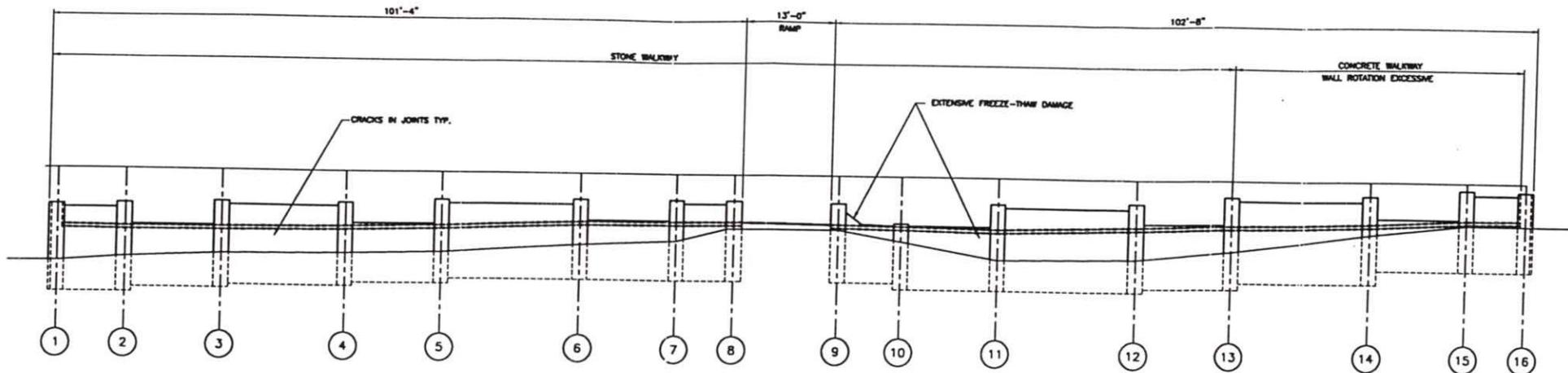
FIELD SURVEY OBSERVATIONS

ELEVATIONS

Date
DEC. 99

Project No.
MJBA 9919

Sheet No.
FS.2



ELEVATION (LOOKING EAST)

FIELD MEASUREMENTS

PIER NUMBER	HT TO GRADE (INCHES)	HT TO WALK (INCHES)	PLUMP (INCHES)	ROTATION (PERCENT)
PIER 1	99"	36"	2" WEST	2.0%
PIER 2	94"	40"	1.25" WEST	1.3%
PIER 3	91"	44"	1.5" WEST	1.7%
PIER 4	86"	41"	1.25" WEST	1.4%
PIER 5	91"	44"	0.5" WEST	0.8%
PIER 6	79"	36"	1.5" WEST	1.8%
PIER 7	71"	36"	1.5" WEST	2.1%
PIER 8	48"	37"	1" WEST	2.1%
PIER 9	46"	37"	0.5" WEST	1.1%
PIER 10	BROKEN			
PIER 11	97"	44"	2" WEST	2.1%
PIER 12	97"	41"	2" WEST	2.1%
PIER 13	95"	50"	4" WEST	4.2%
PIER 14	88"	48"	4.25" WEST	6.2%
PIER 15	81"	53"	3" WEST	4.8%
PIER 16	98"	50"	3.25" WEST	5.5%

NOTES:

- DRAWING BASED ON FIELD MEASUREMENTS OF EXPOSED STRUCTURE.
- ALL DIMENSIONS ARE BASED ON APPROXIMATE PROFILE OF STONE FACE.
- BOTTOM OF RETAINING WALL ASSUMED 4'-6" BELOW GRADE.
- EXCESSIVE WALL MOVEMENT MAY BE CAUSED BY POOR BACKFILL OR DRAINAGE.
- CONSTRUCTION ESTIMATE ASSUMES REPLACING WALL AND ADDING DRAINAGE.
- REPLACEMENT WALL ASSUMES SALVAGING EXISTING STONE FOR NEW VENEER.

0 TOP -- 11/16/99 ISSUED TO CLIENT NO. BY CHK. APP. DATE REVISION DESCRIPTION		CLIENT: 11/99 BARR CONSTRUCTION APPROVAL:		Project Office: BARR BARR ENGINEERING CO. 555 WEST 27TH STREET HOBBS, MN 55746 Concrete Reinforcing Steel Minneapolis, Minnesota Ph: 1-800-822-1000 Fax: (218) 362-3480 www.barr.com		Scale: 1/8" = 1'-0" Date: 10/05/99 Drawn: TOP Checked: Redlined: TOP Approved:		M. J. BURNS ARCHITECTS, LTD. MOORHEAD, MINNESOTA		MN/DOT HISTORIC SITES SURVEY ORR, MINNESOTA ORR ROADSIDE PARKING AREA FIELD INSPECTION		BARR PROJECT No. 23/69-547-TOP CLIENT PROJECT No. 9919 DWG. No. FF-001 REV. No. 0	
		RELEASED TO/FOR: A B C D O 1 2 3 DATE RELEASED:											



1. Parking Lot looking West across TH 53



2. View of Overlook Wall and Site from Fishing Dock looking East
(note: north half of wall is obscured by heavy vegetation)



3. View of south half of overlook with trail and vegetation



4. Looking South from the Overlook toward the Picnic Area
(note: the trail and boulders in the foreground)



5. View of the Picnic Shelter, parking lot, fireplace and picnic table at the South site



6. Looking North toward the Fishing Dock from the Picnic Area



7. South curved Wall Section (note: vertical board at the far left pier is placed to partially cover the space between the corner joint)



8. View of Southwest Wall showing missing stones



9. View of Gravel Ramp and Collapsed Section of Wall



10. Space in SW corner Wall where straight wall is rotating and leaning toward the lake



11. North Curved Wall Section (note: missing stone and overgrown vegetation along guard rail beyond)



12. Flagstone Walk (note: settling and excess vegetation)



13. Looking South toward the rotating (leaning) Wall section



14. Existing concrete parking lot guardrails @ Picnic Area



15. Flagstone Walk looking South



16. Top of Wall (*note: excess moss growth*)



17. Existing Light Post Concrete Base at Picnic Area (South)

SITE BOUNDARIES

■ BOUNDARY OF NATIONAL REGISTER-LISTED PROPERTY

The boundary of the National Register-listed property is shown by the line on the accompanying sheets entitled "Orr Roadside Parking Area Site Boundaries." The base maps for these sheets are a Minnesota Department of Transportation (Mn/DOT) Right-of-way Map and a Mn/DOT aerial photo.

The northern, eastern, and southern boundaries of the National Register-listed property are generally aligned with Mn/DOT right-of-way lines. The western boundary follows the shoreline of Pelican Lake (which is also the Mn/DOT right-of-way line) except where the western boundary extends into the lake to encompass the park's Fishing Dock in a 75' by 150' rectangle, as shown.

Boundary Justification

The boundary of the National Register-listed property generally conforms to the historical boundary of the roadside park as it was envisioned by the Minnesota Department of Highways in early construction plans.

■ RECOMMENDED BOUNDARY OF MN/DOT HISTORIC SITE CONSERVATION ZONE

The recommended boundary of the Mn/DOT Historic Site Conservation Zone is also shown on the accompanying sheets. The Conservation Zone encompasses both the National Register-listed property, marked by the dashed line, and adjacent areas marked by the solid line.

Boundary Justification

The Mn/DOT Historic Site Conservation Zone is recommended to provide a special management zone that includes both the National Register-listed site and a larger area that encompasses part of the historic property's early physical and visual "context" or setting.

Preserving the property's physical and visual setting will help protect its historic integrity and enhance the public's understanding of, and appreciation for, the historic site design. The Conservation Zone will help buffer the site from elements that may detract from its historic character.

It is recommended that the Conservation Zone boundaries include the National Register-listed property and additional land described as follows:

South of the National Register-listed property, it is recommended that the Conservation Zone include Mn/DOT right-of-way extending 400' southward. East of the National Register-listed property, it is recommended that the Conservation Zone extend 40' east of the listed property. North of the National Register-listed property, it is recommended that the Conservation Zone include Mn/DOT right-of-way and state forestry station land extending northward to the Mn/DOT right-of-way line on the northern side of the trunk highway, as shown.

It is recommended that Mn/DOT retain all current right-of-way within the Conservation Zone. It is further recommended that Mn/DOT preserve the Conservation Zone by taking such actions as special

right-of-way planting and maintenance, acquiring additional property or scenic easements, and/or creating partnership agreements with individuals or groups interested in preserving the historic property and its setting. The Mn/DOT Cultural Resources Unit should be consulted regarding these activities.

In particular, it is recommended that Mn/DOT, the City of Orr, and the MnDNR work together closely to preserve and maintain the roadside park, the Conservation Zone, and the entire setting in a manner consistent with the historical design intent. Historic plans and photos should be used to guide treatment activities.

The Orr Roadside Parking Area serves as Orr's only city park.

■ **MORE INFORMATION**

For detailed information on the Orr Roadside Parking Area's structures, landscape, and significance, refer to:

Mn/DOT Historic Roadside Development Structures Inventory form for Orr Roadside Parking Area (Gemini Research, Dec. 1998).

Mn/DOT Historic Roadside Development Structures Preservation and Restoration Report for Orr Roadside Parking Area (Michael J. Burns Architects and Gemini Research, 2001).

National Register Nomination Form for Orr Roadside Parking Area (Gemini Research, Nov. 2001).

Prepared by Gemini Research May 1, 2004.

Orr Roadside Parking Area
Site Boundaries



— National Register-listed Property
- - Historic Site Conservation Zone
Conservation Zone includes the
National Register Property

0 125'
Scale 1" = 125'

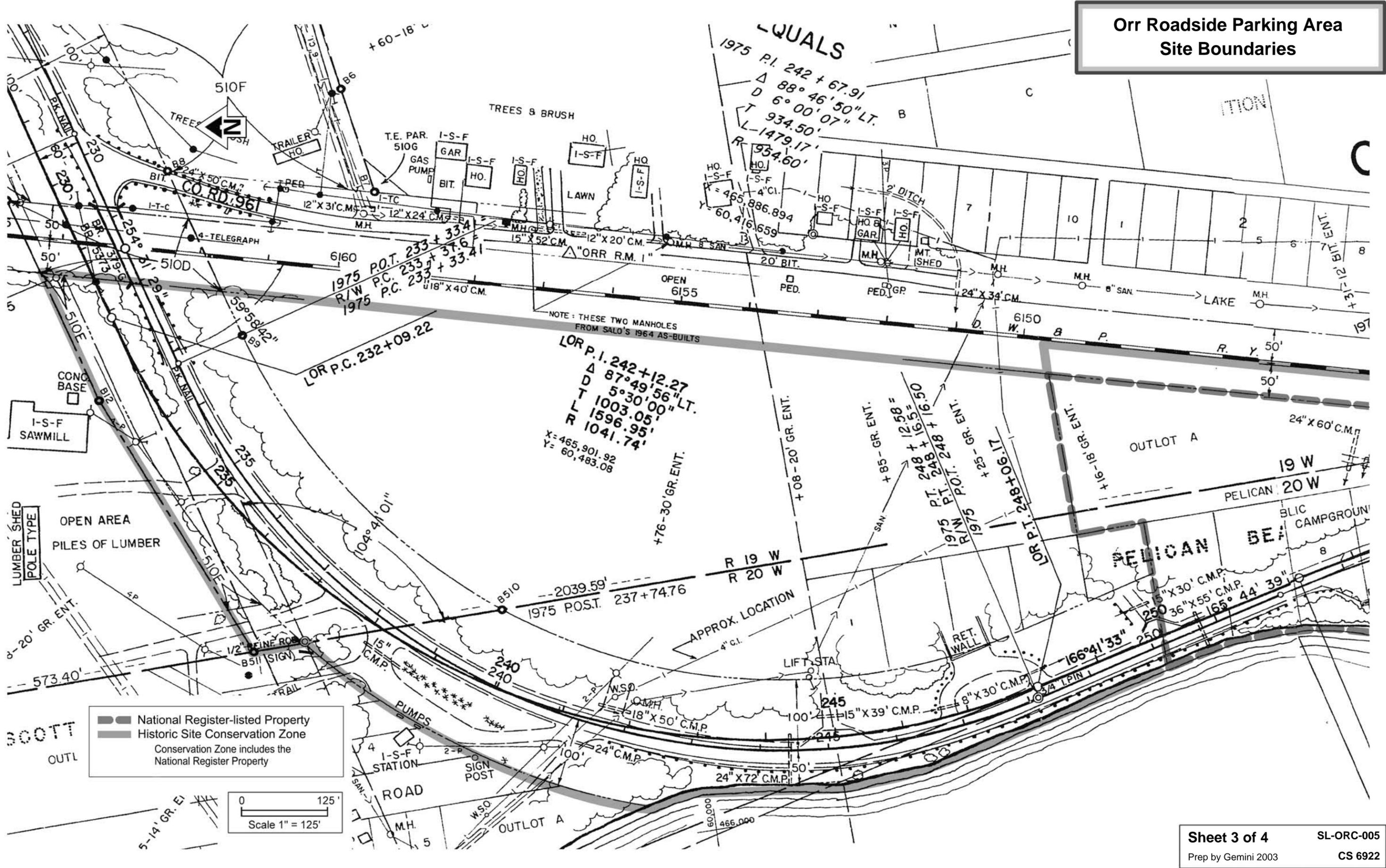
Photo taken Spring 1996

Orr Roadside Parking Area
Site Boundaries

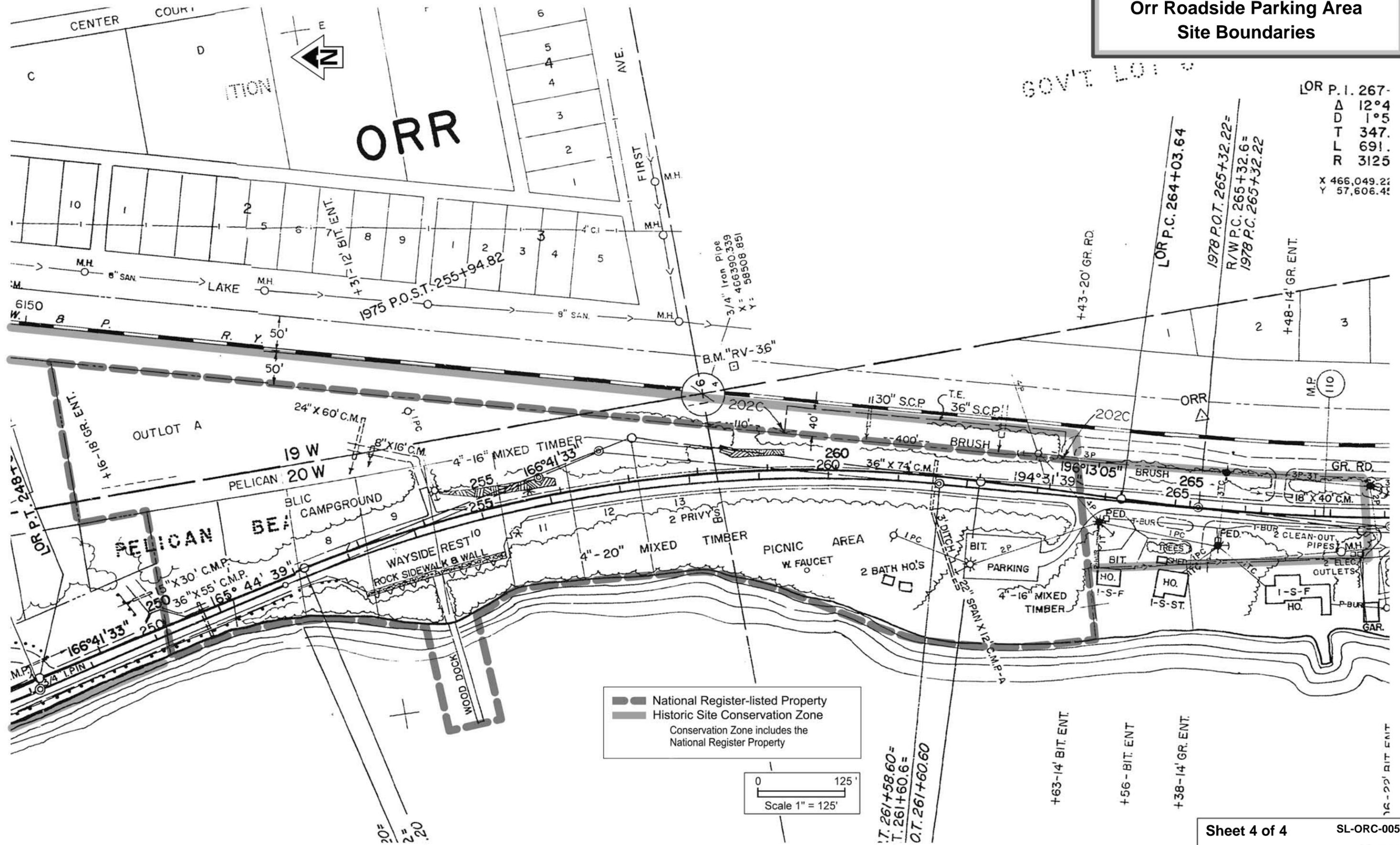


Photo taken Spring 1996

**Orr Roadside Parking Area
Site Boundaries**



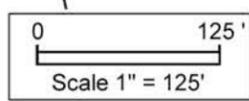
Orr Roadside Parking Area Site Boundaries



GOV'T LOT

LOR P. I. 267-
 Δ 12°4
 D 1°5
 T 347.
 L 691.
 R 3125
 X 466,049.22
 Y 57,606.42

National Register-listed Property
 Historic Site Conservation Zone
 Conservation Zone includes the National Register Property



T. 261+58.60 =
 T. 261+60.6 =
 O.T. 261+60.60

+63-14' BIT. ENT.
 +56- BIT. ENT.
 +38-14' GR. ENT.
 16-22' BIT ENT.